



SEH

Public Involvement Meeting

Project ID 7849-03-04(74)
Globe – East County Line
County K Intersection
County H
Clark County

City of Loyal City Hall
5:30pm – 6:30pm
Thursday, April 13, 2023

Project Contact Information:

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Project Location

- Project begins 530 feet west of County K and ends 1500 feet east of County K.
- Project is 2030 feet or approximately 1/3 of a mile long.

Project Purpose & Need

- The **purpose** of the project is to improve the safety and functionality of the intersection of County H and County K.
- The **need** for the project is due to the high crash rate at the intersection of County H and County K. The five-year crash history (2017-2021) shows six intersection crashes. Of the six crashes, there was one double fatality, two type A (Suspected Serious Injury) and one type C (Possible Injury) severity crashes. County H has poor visibility to the east of the intersection for westbound traffic approaching the intersection. The existing Stopping Sight Distance (SSD) on this section of CTH H is substandard. This project has qualified for WisDOT Highway Safety Improvement Program (HSIP) funding.

Project Overview

- Project consists of reconstructing the existing intersection and the hill on of County H roadway east of the intersection. Making the intersection safer for the traveling public.
- The new roadway corridor will consist of
 - 12-foot lanes, 6-ft shoulder with 5-ft paved
 - Addition of EB and WB right-turn lanes on County H
 - Horizontal alignment will remain unchanged
 - Reduction of vertical profile on County H east of intersection
 - Improved Stopping Sight Distance and Intersection Sight Distance
 - Addition of curb and gutter to reduce impacts to properties, cemetery and environment



Jct CTH K @ CTH H Clark Co _ York Township _ 1/18/2022

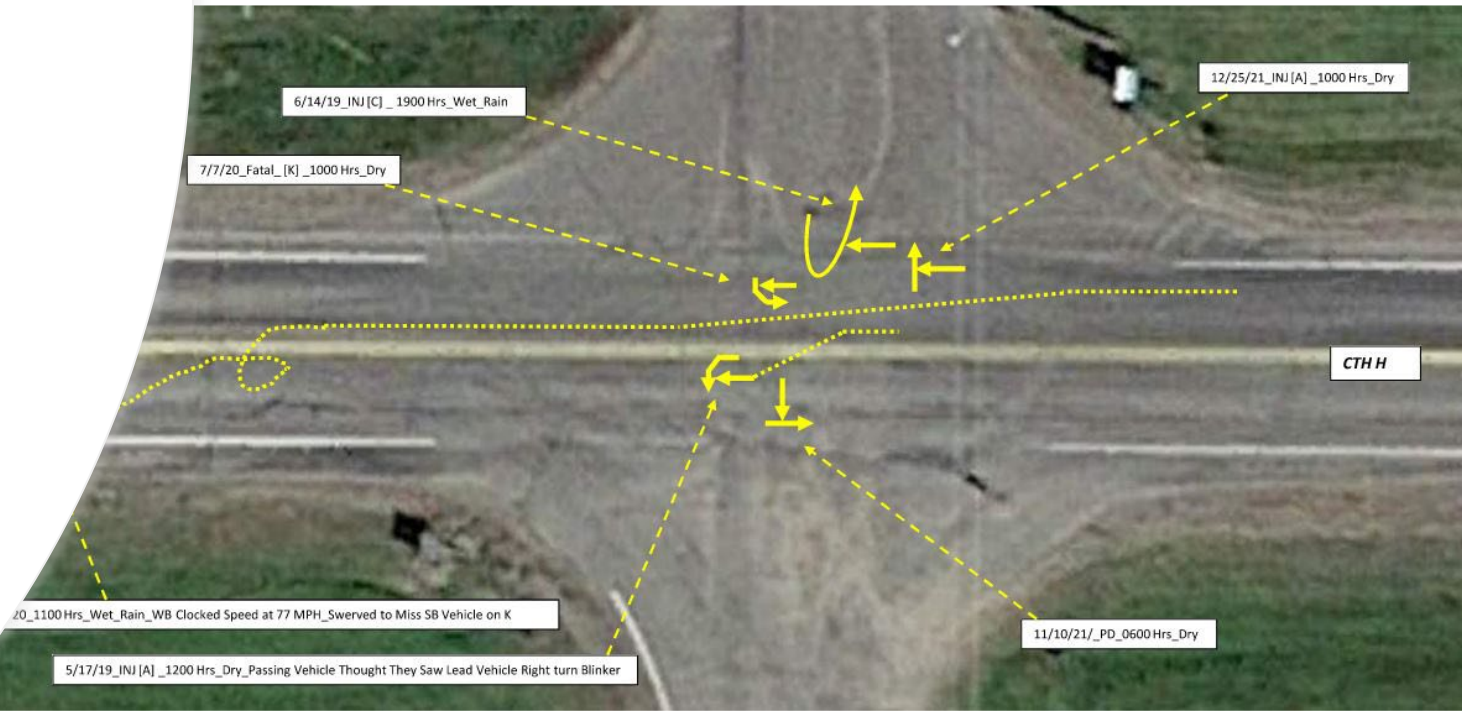
CTH H/CTH K– Safety Concerns

- CTH H has an AADT* of 2100 VPD** (2021)
- CTH K has an AADT* of 1300 VPD** (2019)
- 6 crashes occurred between 2017-2021
 - 1 double fatality, 2 Type A Injury, 1 Type C Injury, and 2 Property Damage crashes
- Proposed improvements would target 4 of 6 intersection crashes, including all fatal/injury crashes. Overall reduction of crashes of 37.8%

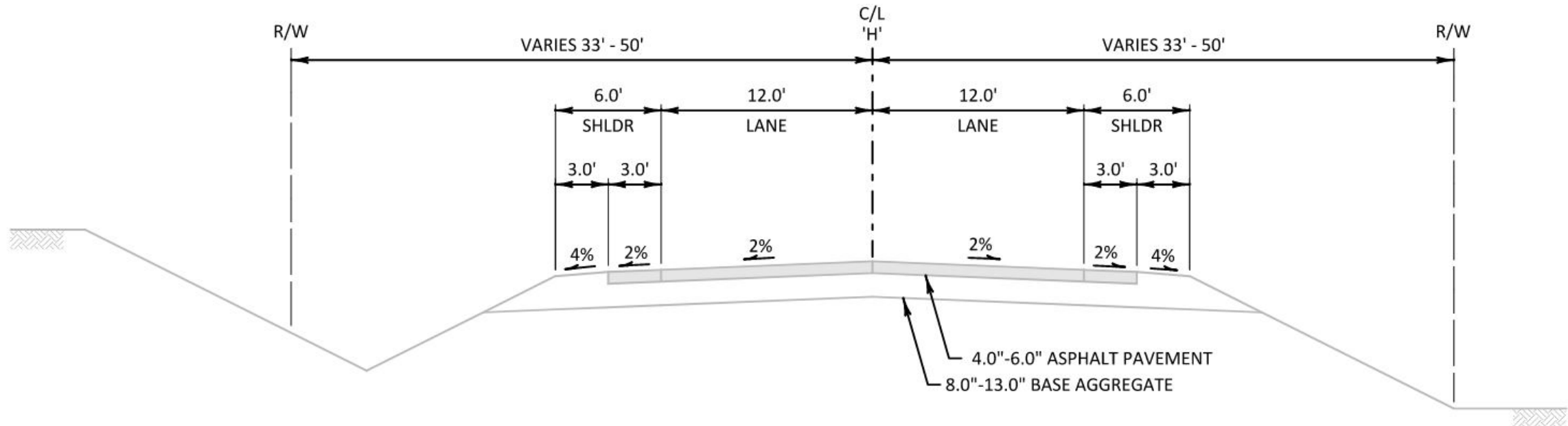
*Annual Average Daily Traffic (AADT)

**Vehicles Per Day (VPD)

										INJ	INJ	INJ	
Total	Right Angle	Left	DTC	Rear	ROR	SSS	SSOP	Misc	Fatal	A	B	C	PD
0	0	0	0	0	0	0	0	0	0	0	0	0	0
0	0	0	0	0	0	0	0	0	0	0	0	0	0
2	2	0	0	0	0	0	0	0	0	1	0	1	0
2	1	0	0	0	1	0	0	0	1	0	0	0	1
2	2	0	0	0	0	0	0	0	0	1	0	0	1
5	5	0	0	0	1	0	0	0	1	2	0	1	2



Existing Typical Section – CTH H

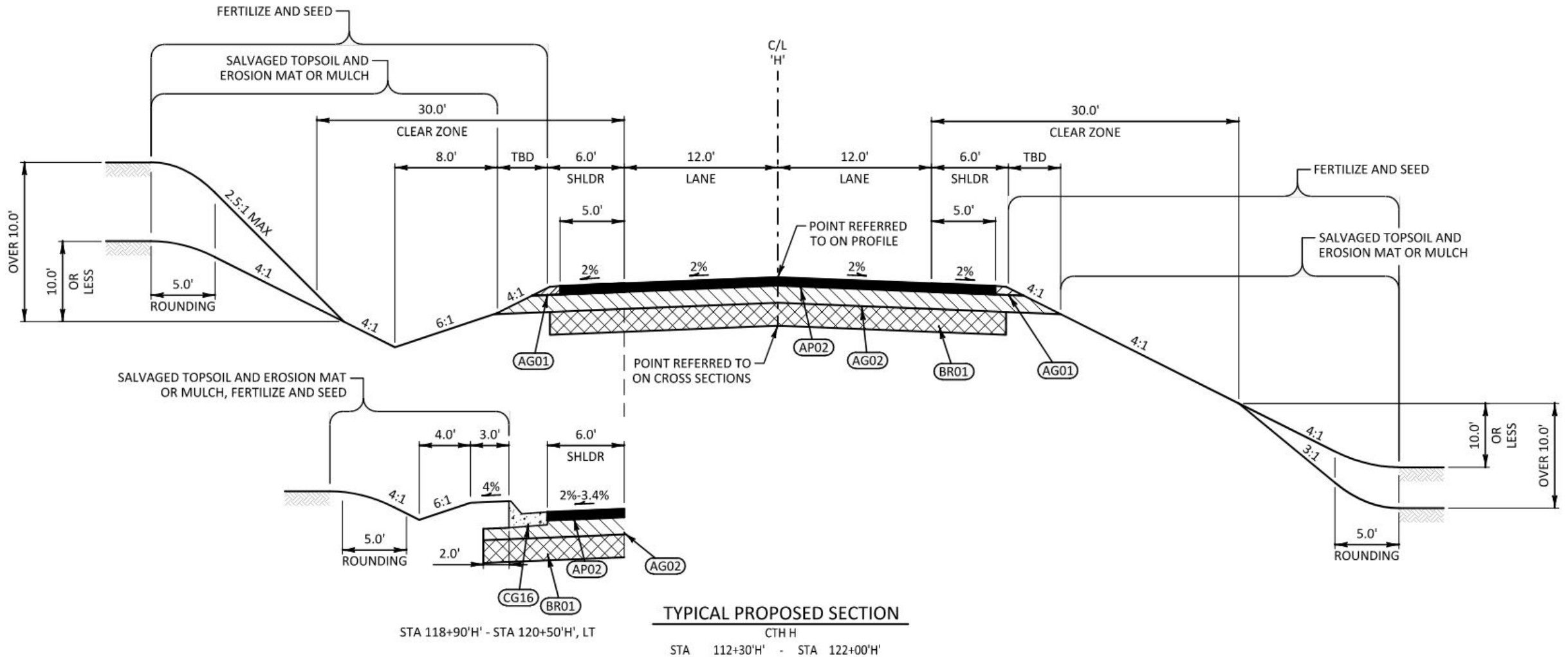


TYPICAL EXISTING SECTION

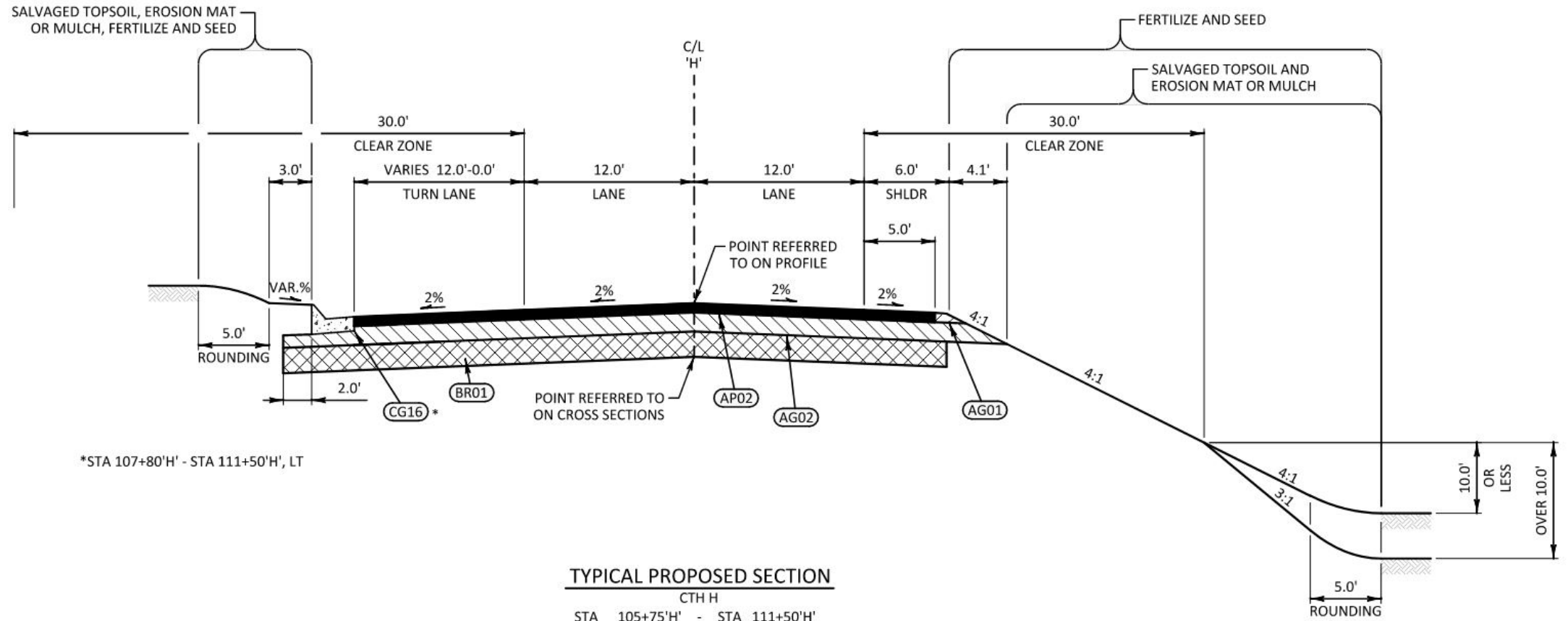
CTH H

STA 101+71'H' - STA 122+00'H'

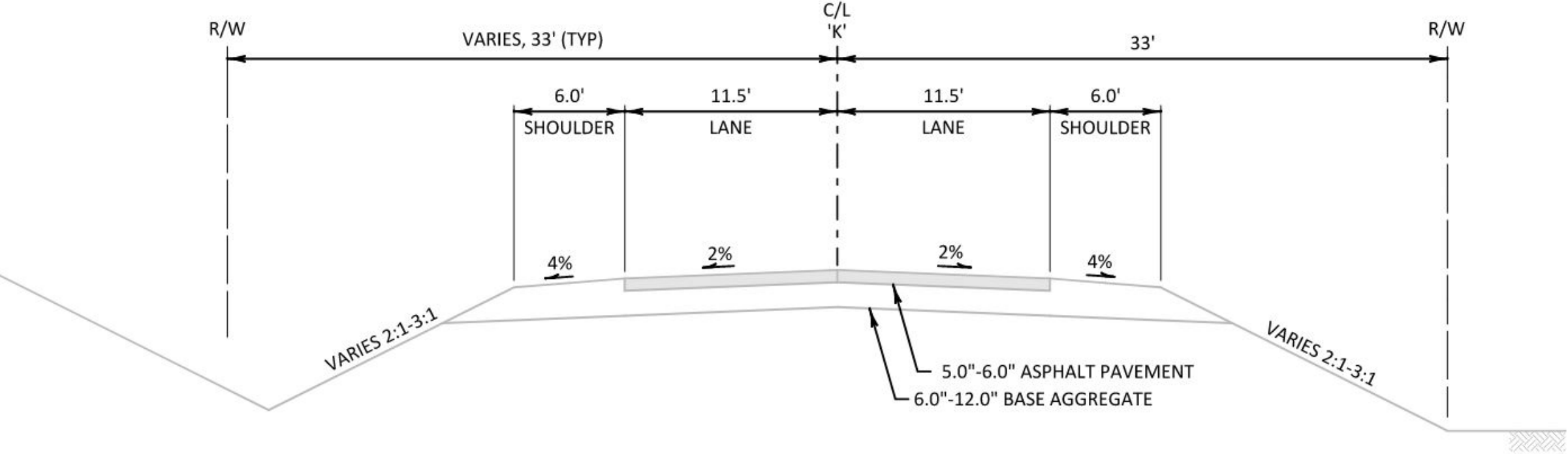
Proposed Typical Section – CTH H



Proposed Right Turn Lane Section – CTH H



Existing Typical Section – CTH K

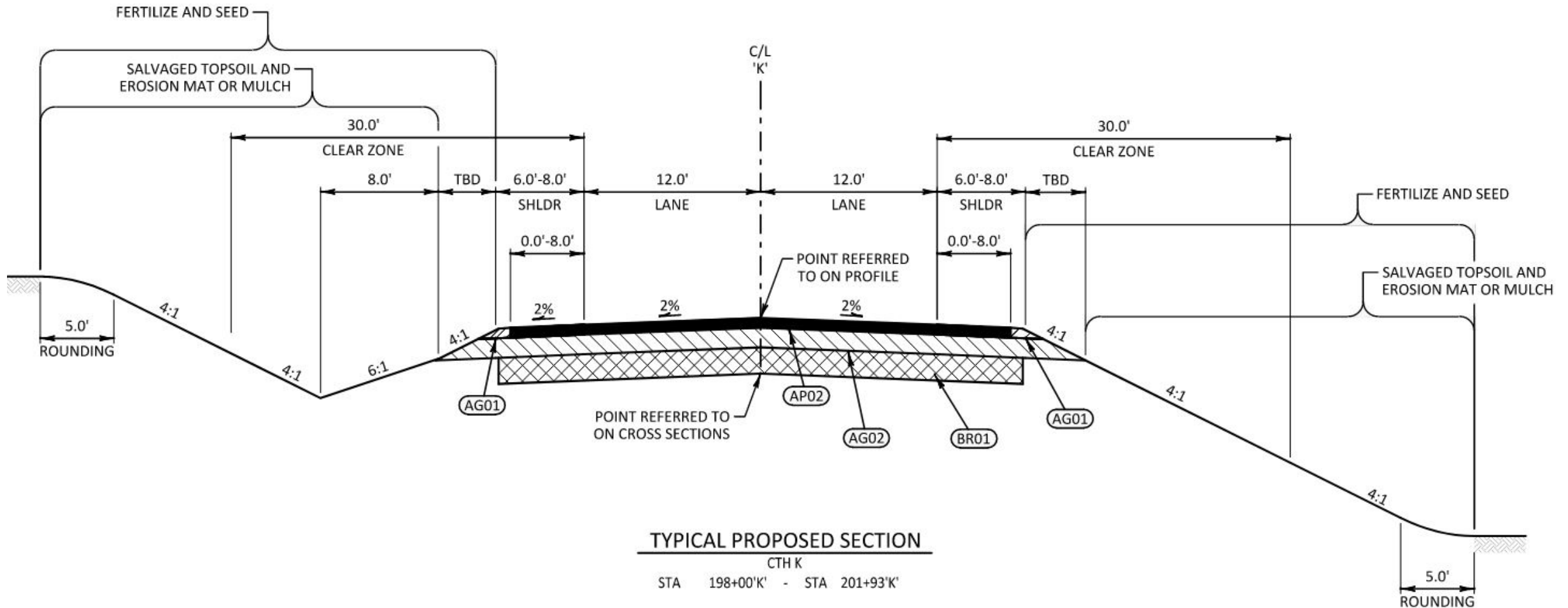


TYPICAL EXISTING SECTION

CTH K

STA 198+00'K' - STA 201+93'K'

Proposed Typical Section – CTH K





Intersection Reconstruction

- Addition of EB and WB right-turn lanes on County H
- Addition of curb & gutter at intersection and ROW areas
- Reduction of vertical profile on County H east of intersection

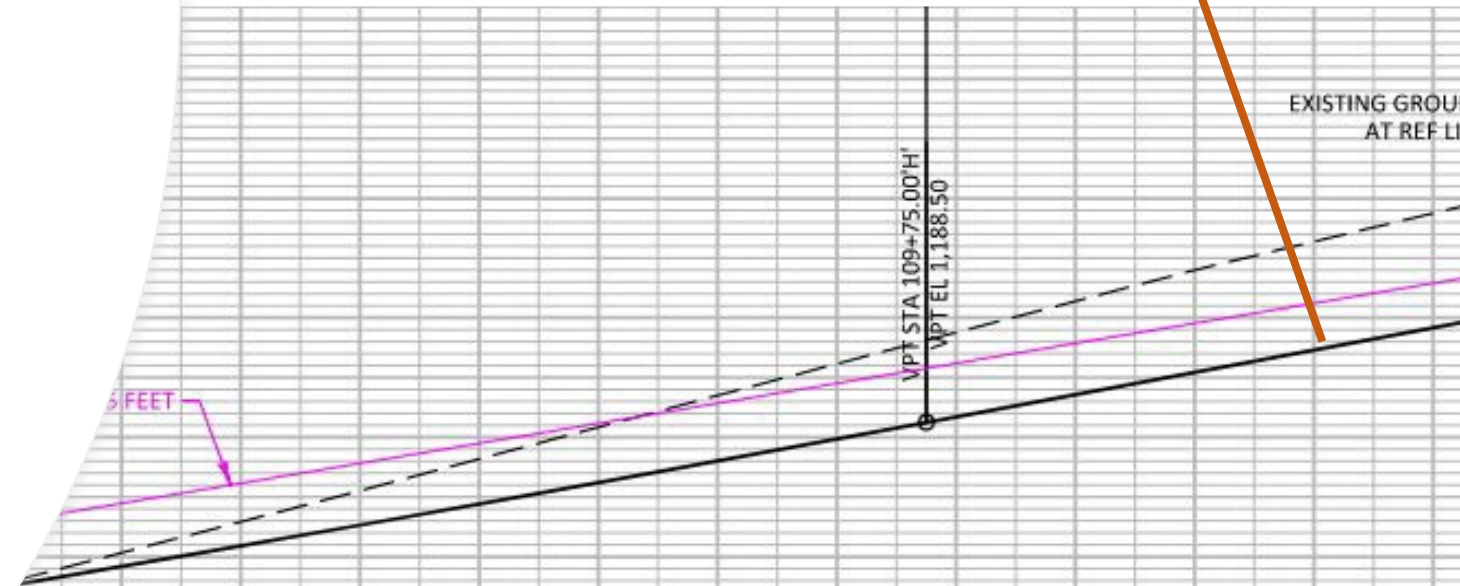


Intersection Reconstruction

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SSD, ISD Improvements

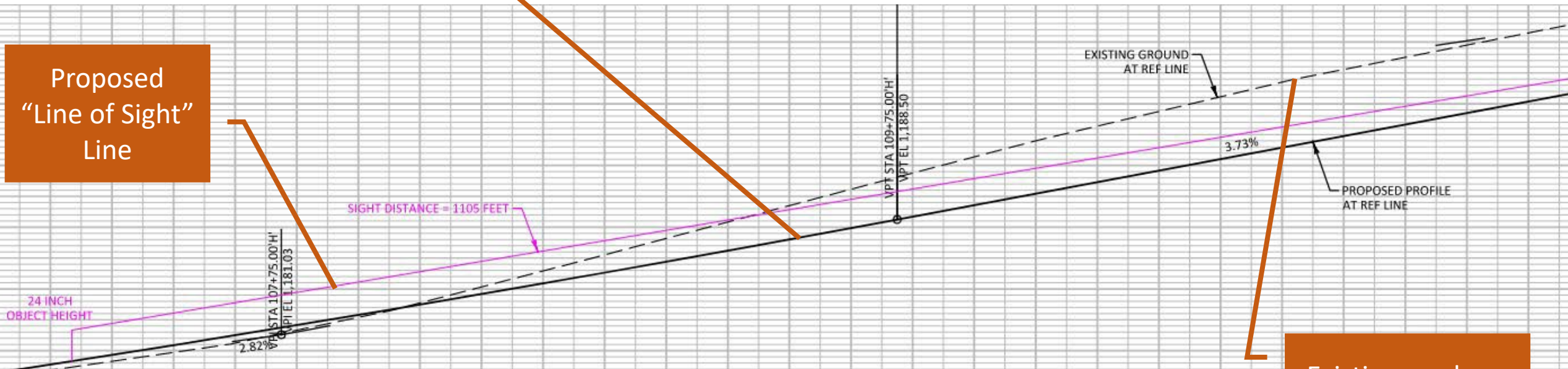
- The proposed reduction in profile on County H of 4 feet to 5 feet will improve the Stopping Sight Distance (SSD) from 493 feet to 594 feet. The standard condition is 570 feet for a design speed of 60 mph and this section of County H is only meeting a design speed of 50 mph.
- The Intersection Sight Distance (ISD) will improve from 1120 feet for the existing condition to 1205 feet for the proposed alternative for northbound vehicles on County K making left-turns onto County H. The standard condition is 1120 feet.
- The Decision Sight Distance (DSD) will improve from 800 feet for the existing condition to 1105 feet. The standard condition is 990 feet. The DSD allows a driver to recognize a dangerous situation and complete an avoidance maneuver safely.



Proposed Profile Change

Proposed roadway surface

Proposed "Line of Sight" Line

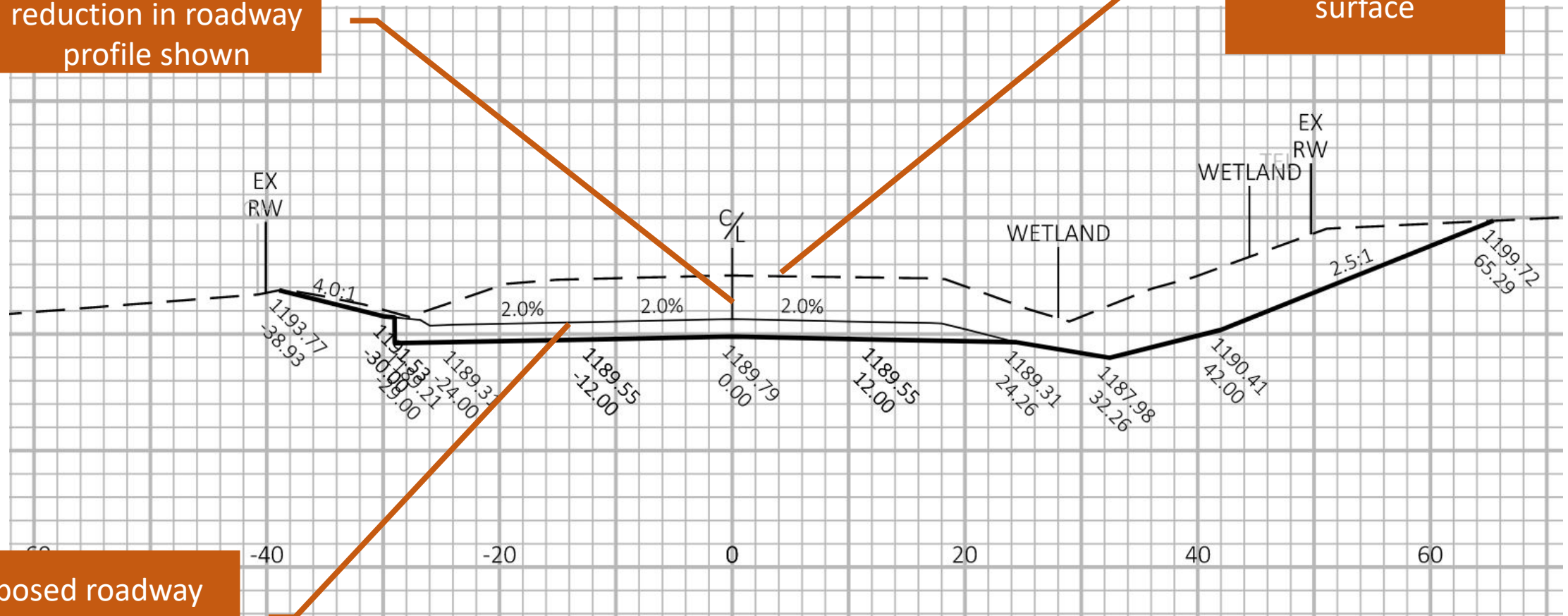


Existing roadway surface

Proposed Profile Change

Approximate 4-ft reduction in roadway profile shown

Existing roadway surface

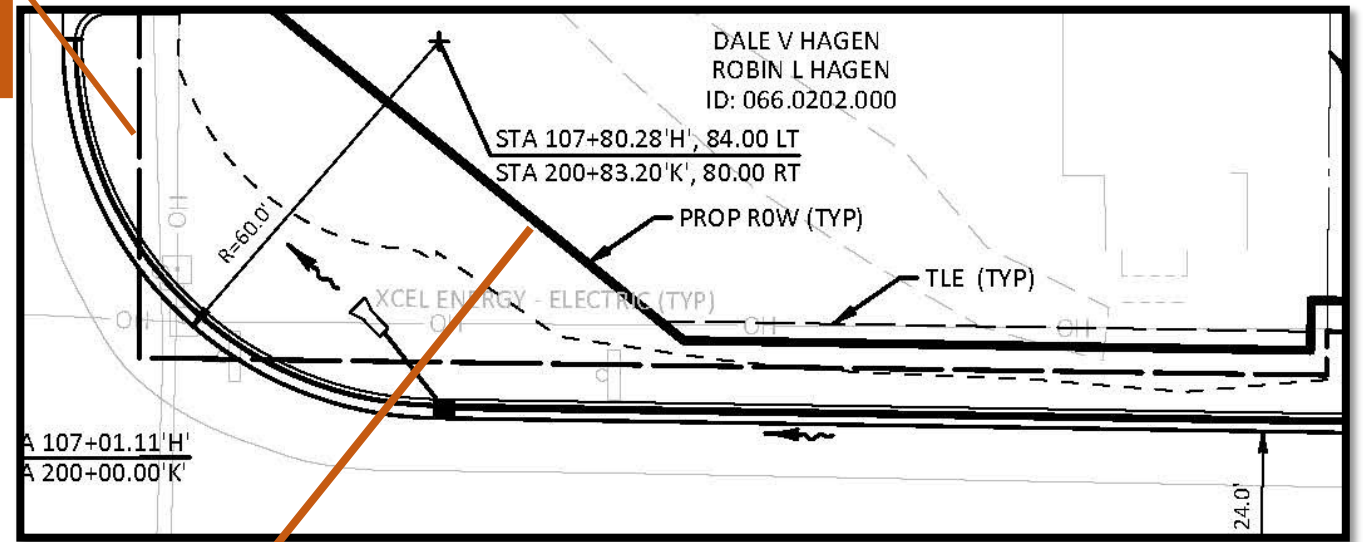
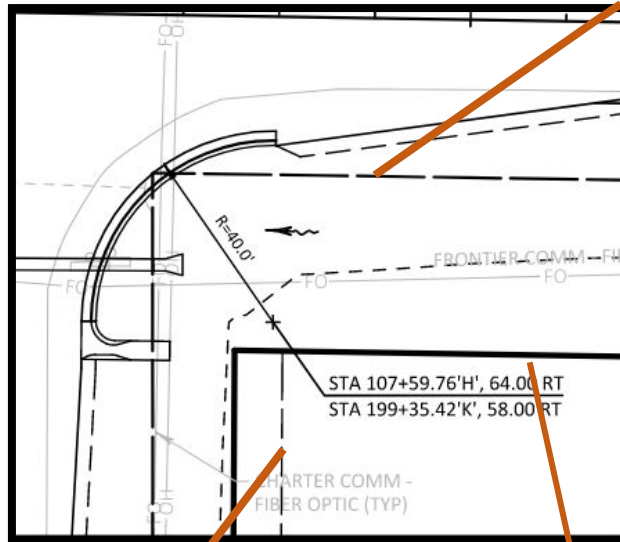


Proposed roadway surface

Real-Estate Process

- Right-of-Way (ROW) Plat
 - Created by SEH based on design needs
 - Temporary and Permanent right-of-way acquisition is anticipated along the project corridor
- Real estate appraisals/acquisition
 - County funded
 - Third party consultant hired to handle appraisals/acquisition
 - Property owner will work directly with third party consultant
 - Federal acquisition process
 - Cannot begin until after the Environmental Document and Design Study Report have been completed and approved

Existing
ROW



Example
TLE ROW

Example FEE ROW

Construction Impacts to Roadway Users

- Roadway will be closed to traffic
 - Detour route will be provided
- Property owners
 - Access maintained
- Emergency Services
 - Access maintained



Anticipated Detour Routes

- Signed detour routes will utilize the following roadways
 - County Y, WIS 98, WIS 73, US 10
- Locals will likely utilize preferred local roadways



Project Costs

- Construction Costs
 - Estimated construction cost \$1.2M - \$1.5M
 - 90% State/Federal Funding
 - 10% Clark County
- Real Estate Costs
 - 100% Locally Funded
 - Clark County
- Utility Relocation Costs
 - 100% Locally Funded
 - Clark County



Overall Project Timeline

JAN 2023 – MAY 2023

- Topographic survey
- Agency/utility coordination
- 30% Design
- Public Involvement Meeting
- Environmental Documentation

JUNE 2023 – AUGUST 2023

- 60% Design
- Design Study Report
- Traffic Management Plan
- Preliminary Right of Way Plat

SEPT 2023 – FEB 2024

- Final Right of Way Plat
- 90% Design
- Final Utility Coordination

MARCH 2024 – NOV 2024

- 90% Design Complete May 2024
- Final Design August 2024
- November 12, 2024

MAY 2025 – NOV 2025

- Anticipated Construction Timeframe

Thank You!
Questions?

